

The Process



Funding the Project

The plan is sponsored by the City of O'Fallon and East-West Gateway Council of Governments.

It is funded, in part, through a grant from the U.S. Department of Transportation through the Illinois Department of Transportation. The process fully complies with Title VI of the Civil Rights Act of 1964 and related statutes and regulations in all programs and activities.

For more information, or to obtain a Title VI Nondiscrimination Complaint Form, see www.ewgateway.org/titlevi or call (314) 421-4220 or (618) 274-2750.

Purpose of the Project

The City of O'Fallon is studying Highway 50 as a part of the East-West Gateway Council of Government's Great Streets Initiative. This project, called Forward 50, will serve to strengthen the function and appearance of Highway 50 for the O'Fallon community.

The project will:

- ensure the community is connected and supported throughout all the neighborhoods
- understand market demands and potential changes in land use
- identify strategies to improve safety for all modes of travel
- imagine aesthetic improvements to strengthen the area's image and desirability
- ...and more



We Want to Hear From You!

Are there areas that work great?

Are there areas that need special attention?

The Highway 50 corridor provides an environment that

serves user needs,

builds community quality,

and **capitalizes on**

opportunities while

minimizing functional

distress.

SERVES USER NEEDS

Motorists, pedestrians, cyclists and transit users are welcomed and accommodated in the Highway 50 public right-of-way. Accesses to businesses are clear, organized, and wayfinding signage directs residents and visitors alike. Improved stormwater infrastructure and utilities further enhance the Highway 50 environment.

BUILDS COMMUNITY QUALITY

Attractive entranceways and corridors bolster the community's image and vibrant gathering spaces build community identity. Improved connections between neighborhoods, downtown, and community destinations enhances the quality of life for O'Fallon residents.

CAPITALIZES ON OPPORTUNITIES

O'Fallon is a thriving, growing community. Commercial and residential land uses are optimized as new development occurs throughout the corridor.

MINIMIZES FUNCTIONAL DISTRESS

Improvements to the Highway 50 corridor maximize safety for motorists, pedestrians, cyclists and transit users while minimizing negative impacts to overall circulation and congestion. Stormwater management is integrated into the design to reduce flooding and erosion concerns.



AUTO/HIGHWAY ENVIRONMENT

- » Pedestrian/Transit Access
- » Stormwater Management
- » Placemaking/Gateway
- » Land uses
- » Sports Park Connection
- » Hospital Site and Connection
- » State Street Intersection

COMMUNITY STREET

- » Active transportation
- » Street section
- » Infill development
- » Residential edge
- » Small business
- » Elementary school access
- » High school access
- » Streetscape/urban design
- » Redevelopment site
- » Civic quality

WALKABLE DOWNTOWN LINK

- » Walkability
- » Bicycle access
- » Small business environment
- » Streetscape
- » Library connection
- » Civic Plaza
- » Adjacent redevelopment
- » Infill development
- » Community Park frontage

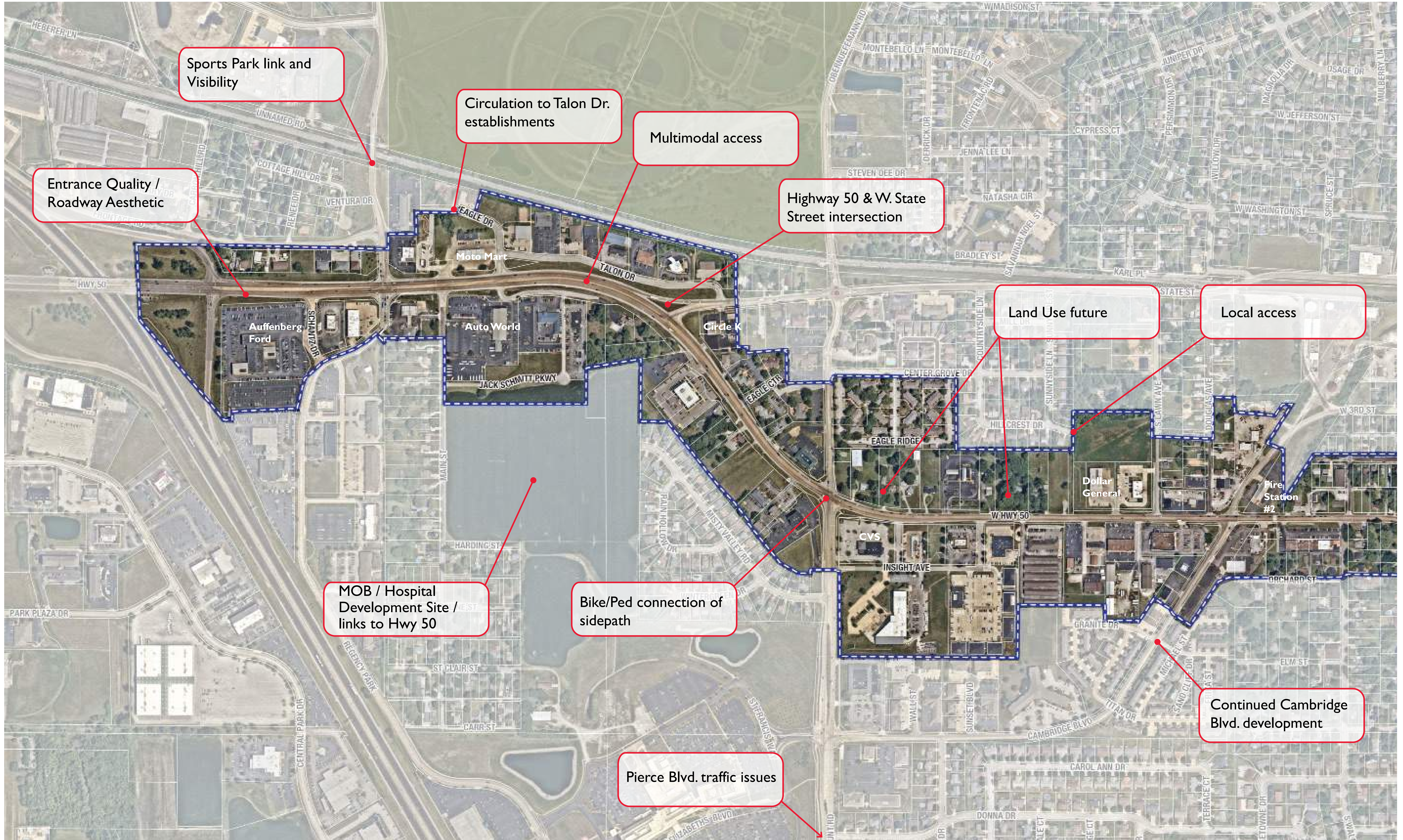
TRANSITION/EDGE

- » Future road section and character
- » Traffic calming
- » Active transportation access
- » New development and city fabric
- » Rec Plex connection
- » Scott Troy intersection
- » City gateway

emerging opportunities: west



WHAT ARE YOUR OBSERVATIONS?



emerging opportunities: central



WHAT ARE YOUR OBSERVATIONS?



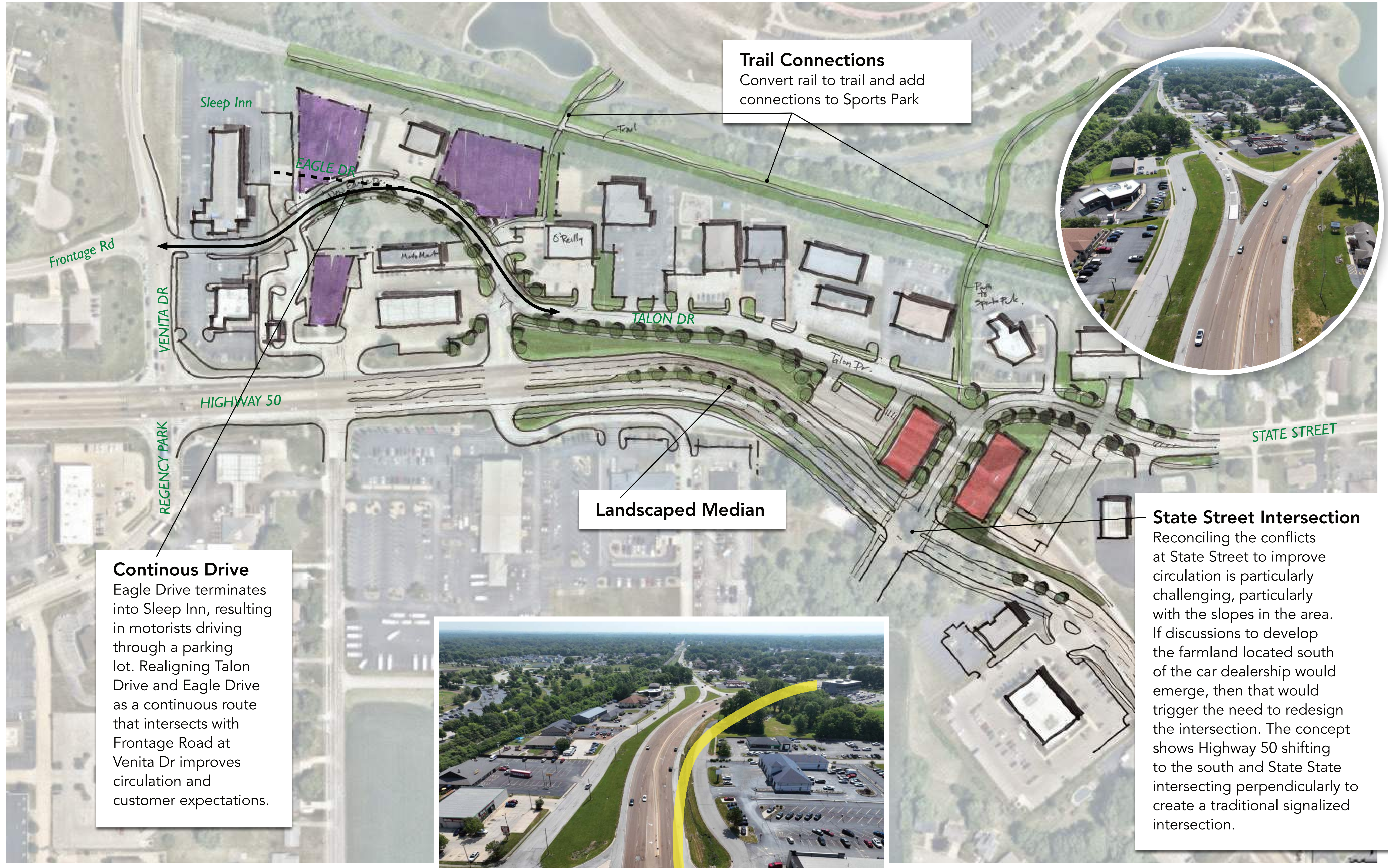
emerging opportunities: east



WHAT ARE YOUR OBSERVATIONS?



50 / Regency Park to State Street



Trail Connections
Convert rail to trail and add connections to Sports Park



Landscaped Median

State Street Intersection
Reconciling the conflicts at State Street to improve circulation is particularly challenging, particularly with the slopes in the area. If discussions to develop the farmland located south of the car dealership would emerge, then that would trigger the need to redesign the intersection. The concept shows Highway 50 shifting to the south and State Street intersecting perpendicularly to create a traditional signalized intersection.

Continous Drive
Eagle Drive terminates into Sleep Inn, resulting in motorists driving through a parking lot. Realigning Talon Drive and Eagle Drive as a continuous route that intersects with Frontage Road at Venita Dr improves circulation and customer expectations.



50 / Green Mt Rd to Lawn Ave



Highway 50 Corridor

Require Development to Connect

Parking areas should connect between development to allow customers to conveniently access businesses without entering/existing Highway 50

Maximize Development Yield

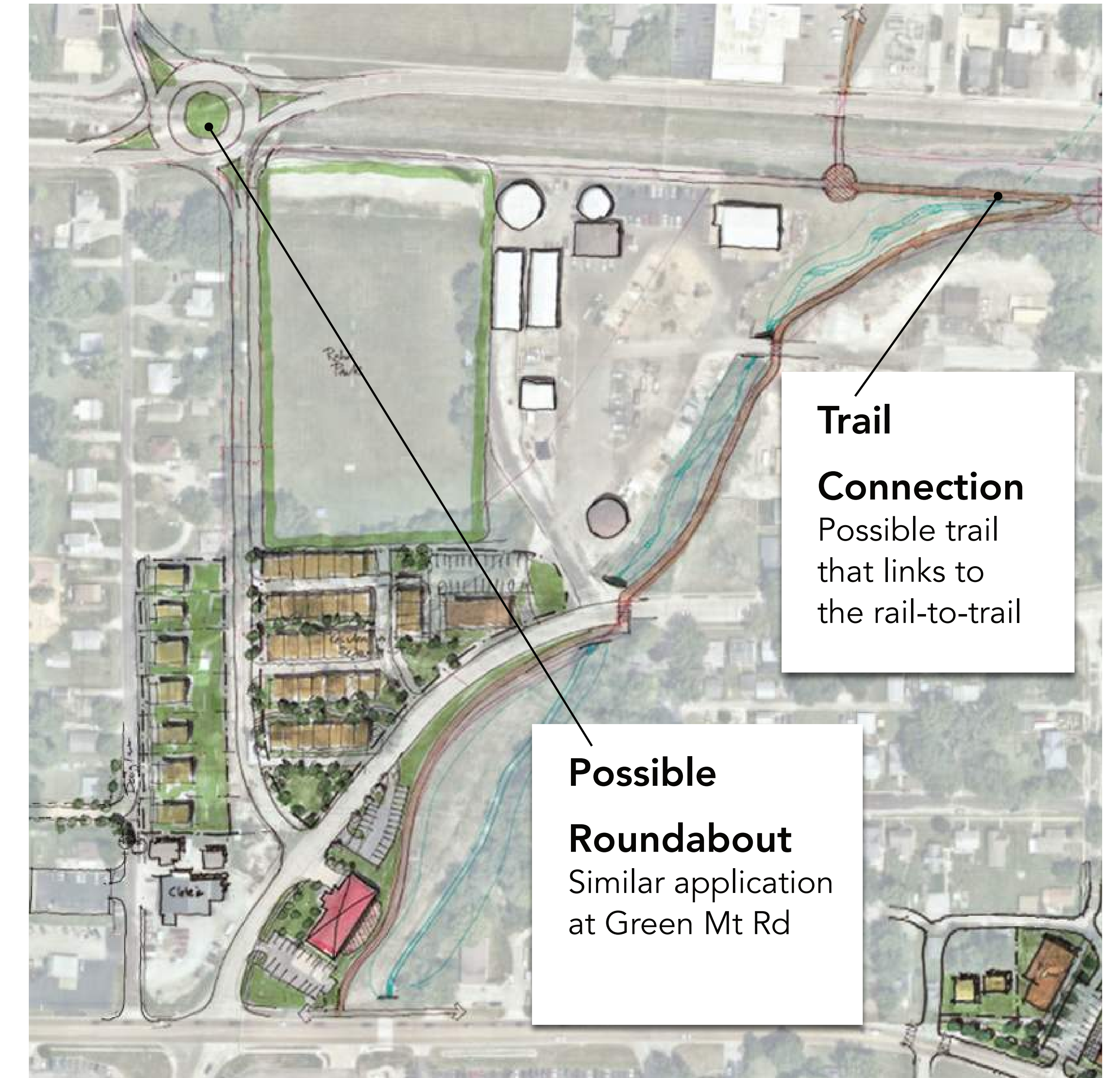
Existing single-family homes fronting the highway can be redeveloped for commercial use. The yield for redeveloping individual lots is marginal compared to redeveloping as a single project.

Development Site

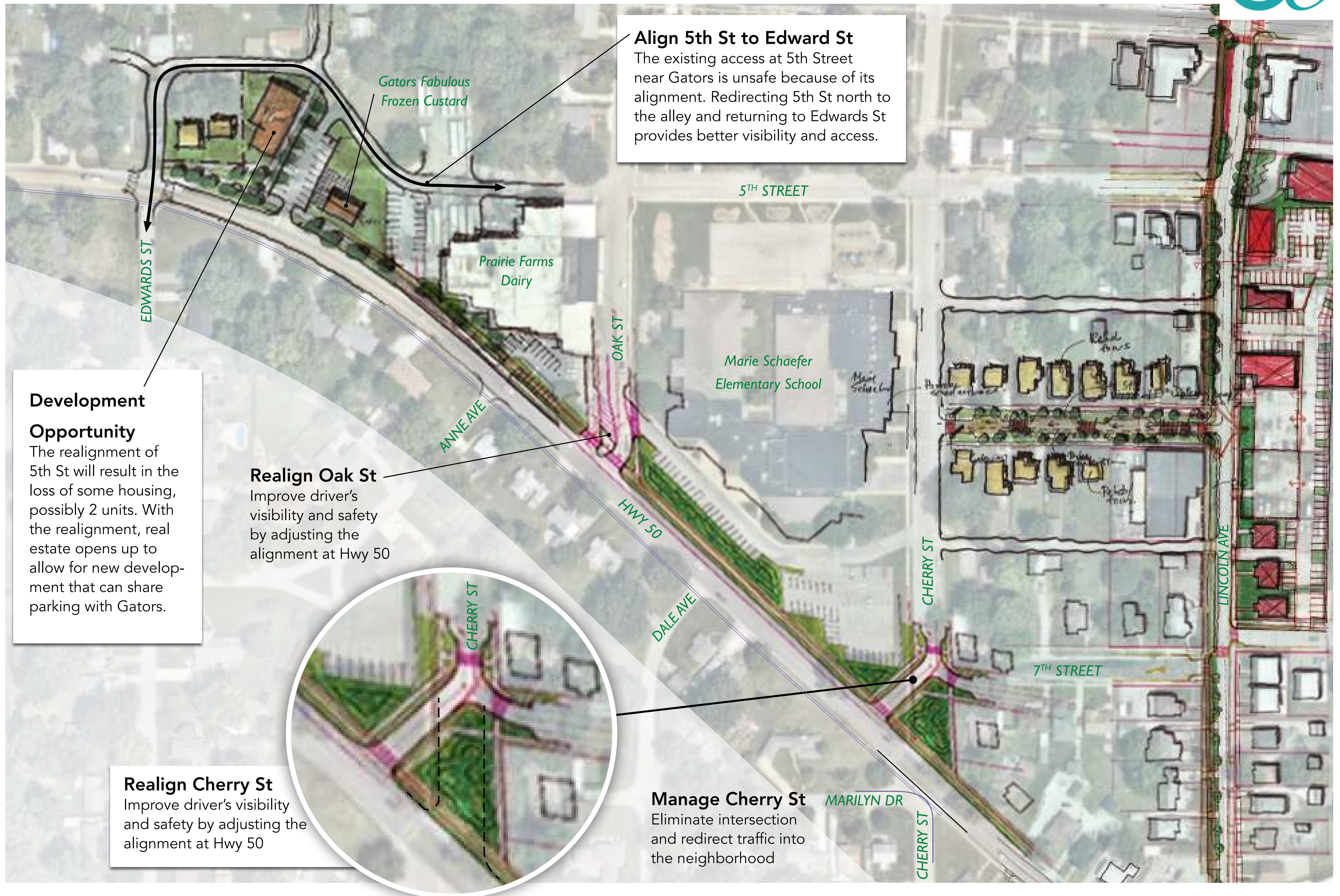
The open site can be master planned to support a mix of uses. Concept shows commercial uses along the highway and multi-family housing transition to the neighborhood.



50 / 3rd Street Environs



50 / Edwards Street to Cherry Street



Align 5th St to Edward St
The existing access at 5th Street near Gators is unsafe because of its alignment. Redirecting 5th St north to the alley and returning to Edwards St provides better visibility and access.

Development Opportunity
The realignment of 5th St will result in the loss of some housing, possibly 2 units. With the realignment, real estate opens up to allow for new development that can share parking with Gators.

Realign Oak St
Improve driver's visibility and safety by adjusting the alignment at Hwy 50

Realign Cherry St
Improve driver's visibility and safety by adjusting the alignment at Hwy 50

Manage Cherry St
Eliminate intersection and redirect traffic into the neighborhood



50 / Southview at Lincoln Avenue



Redevelopment Master Plan for Southview

The Southview Site has been subject to redevelopment proposals. While the Starbucks site plan has been approved for construction, other portions of the site remain uncertain. Prospective developers want a program that results in a return on investment, while neighbors want development that complements their quality of life.

This plan proposes a circulation framework that allows independent projects to emerge, rather than a single major investment. The design of the internal circulation is critical to managing access points on major streets. Deflecting movements discourages traffic from using the network as a shortcut.



Mixed Use



Townhomes



Commercial



50 / Lincoln Avenue



Signature Art Installation

The center of the roundabout is an opportunity to celebrate the community's history and culture through artwork.



Manage Access

Merging access points to Highway 50 improves safety in/out of businesses

Proposed Roundabout

Improve the circulation and safety of the intersection by redesigning it as a roundabout.

Install Wayfinding

Enhance wayfinding along Highway 50 to direct traffic to major destinations in the community.



Lincoln Avenue



Lighting



Integrated artwork and seating areas



Knee walls for screening



Infill Development

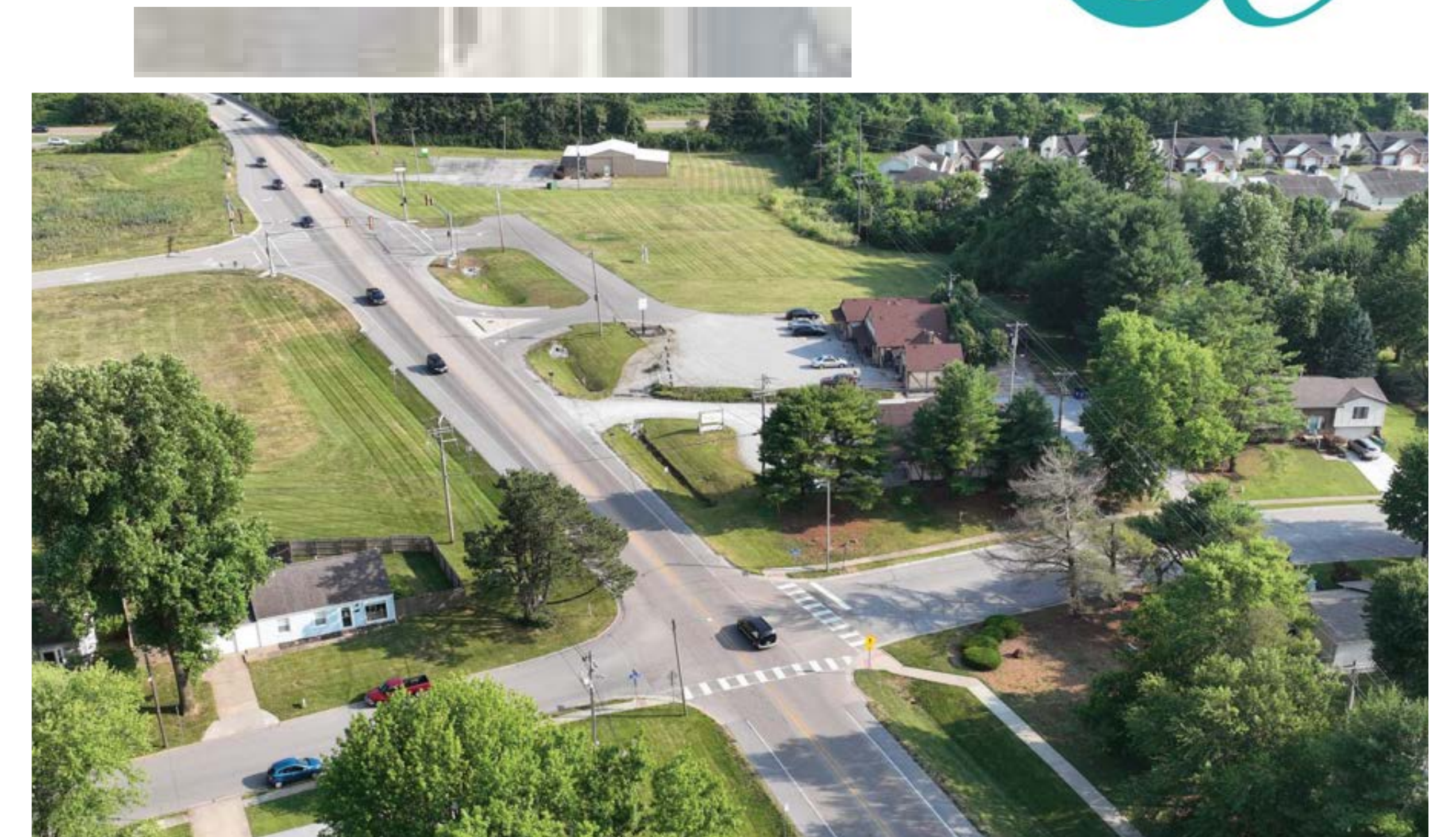
New development should be setback similarly to existing development along the corridor.

Connected Parking Lots

Connecting the parking lots behind existing buildings provides a greater yield and efficiency of parking to businesses.

Strengthen the Edge

While Infill Development is a priority, the redesign of the street should include a knee wall that strengthens the edge of the street, screening the open lots behind.



Lincoln Avenue Sidepath

Widen existing sidewalks and complete gaps on westside of S. Lincoln Avenue, extending south to Tamarak Lane.

Prepare study to complete a wide sidewalk on the eastside as a future phase.

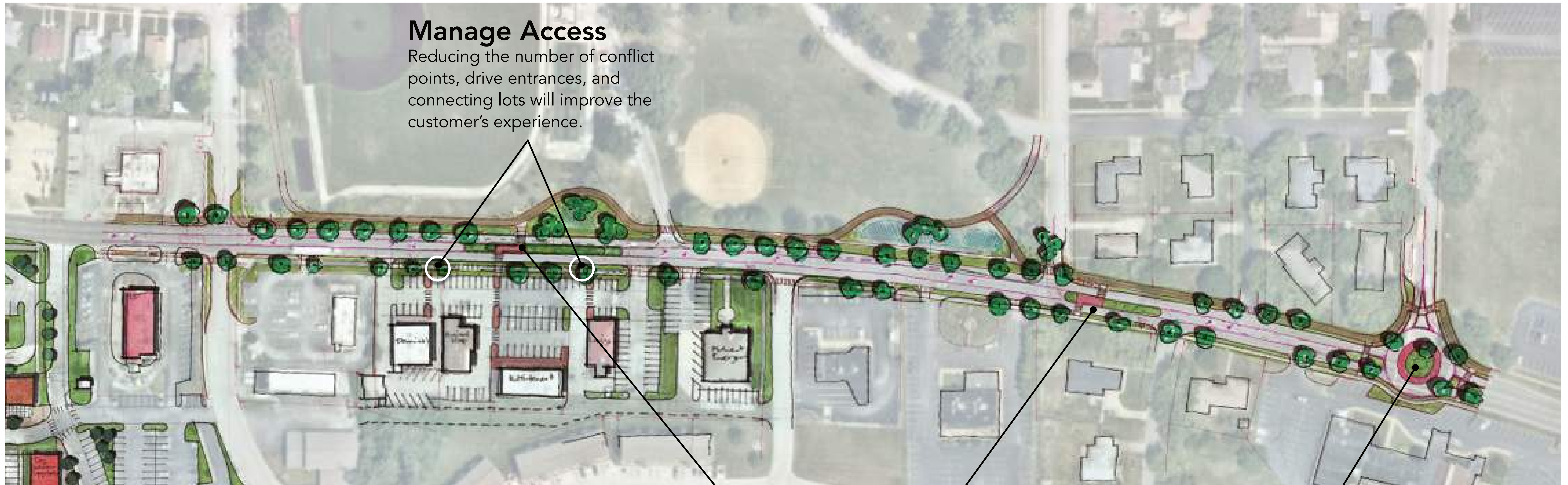
50 / Parkview Drive to Smile Street



O'Fallon Community Park Area

Manage Access

Reducing the number of conflict points, drive entrances, and connecting lots will improve the customer's experience.



Continue Sidewalk

Portions of Highway 50 need sidewalks on both sides to improve accessibility



Build Bus Shelters

Walkways and shelters should be available for transit users.



Pedestrian Refuges

Medians with zigzag walks forces pedestrians to look at oncoming traffic before crossing.



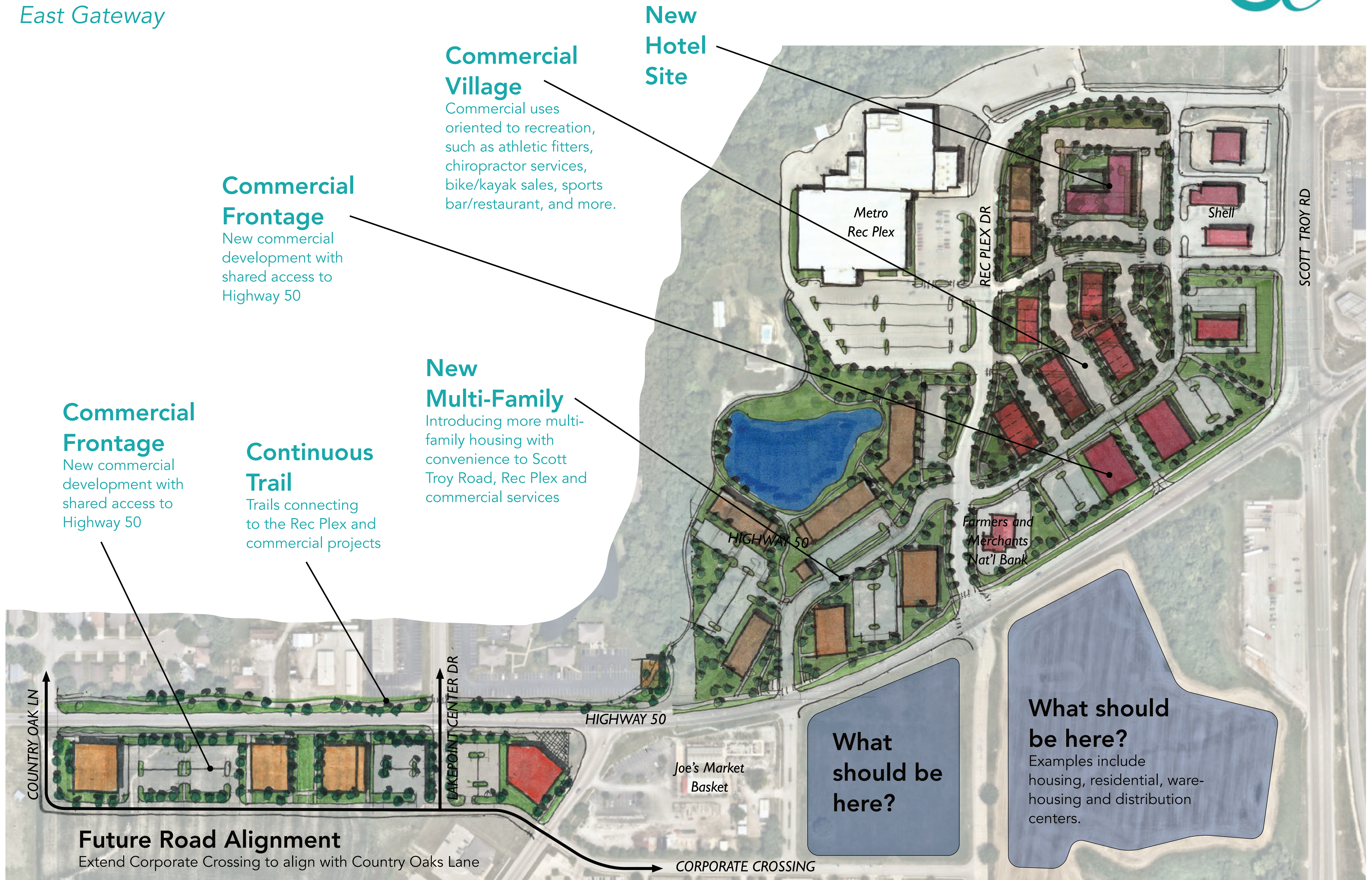
Roundabout & Traffic Calming

A roundabout at Smile Street intercepts westbound traffic, slowing them down when approaching the parks and business district. Also, the roundabout allows traffic to flow better at the end of the school day.

50 / Country Oaks Lane to Scott Troy Road



East Gateway



Commercial Frontage

New commercial development with shared access to Highway 50

Commercial Village

Commercial uses oriented to recreation, such as athletic fitters, chiropractor services, bike/kayak sales, sports bar/restaurant, and more.

New Hotel Site

New Multi-Family

Introducing more multi-family housing with convenience to Scott Troy Road, Rec Plex and commercial services

Continuous Trail

Trails connecting to the Rec Plex and commercial projects

Commercial Frontage

New commercial development with shared access to Highway 50

Future Road Alignment

Extend Corporate Crossing to align with Country Oaks Lane

What should be here?

What should be here?

Examples include housing, residential, warehousing and distribution centers.

street sections



existing



I-94 > State



State > Green Mt



Green Mt > 5th



5th > Lincoln



Lincoln > Smiley



Smiley > Seven Mile



Seven Mile > Rec Plex



Rec Plex > Scott Troy



future